

HORWICH TOWN CENTRE CONSERVATION AREA



Planning and Engineering Services Department

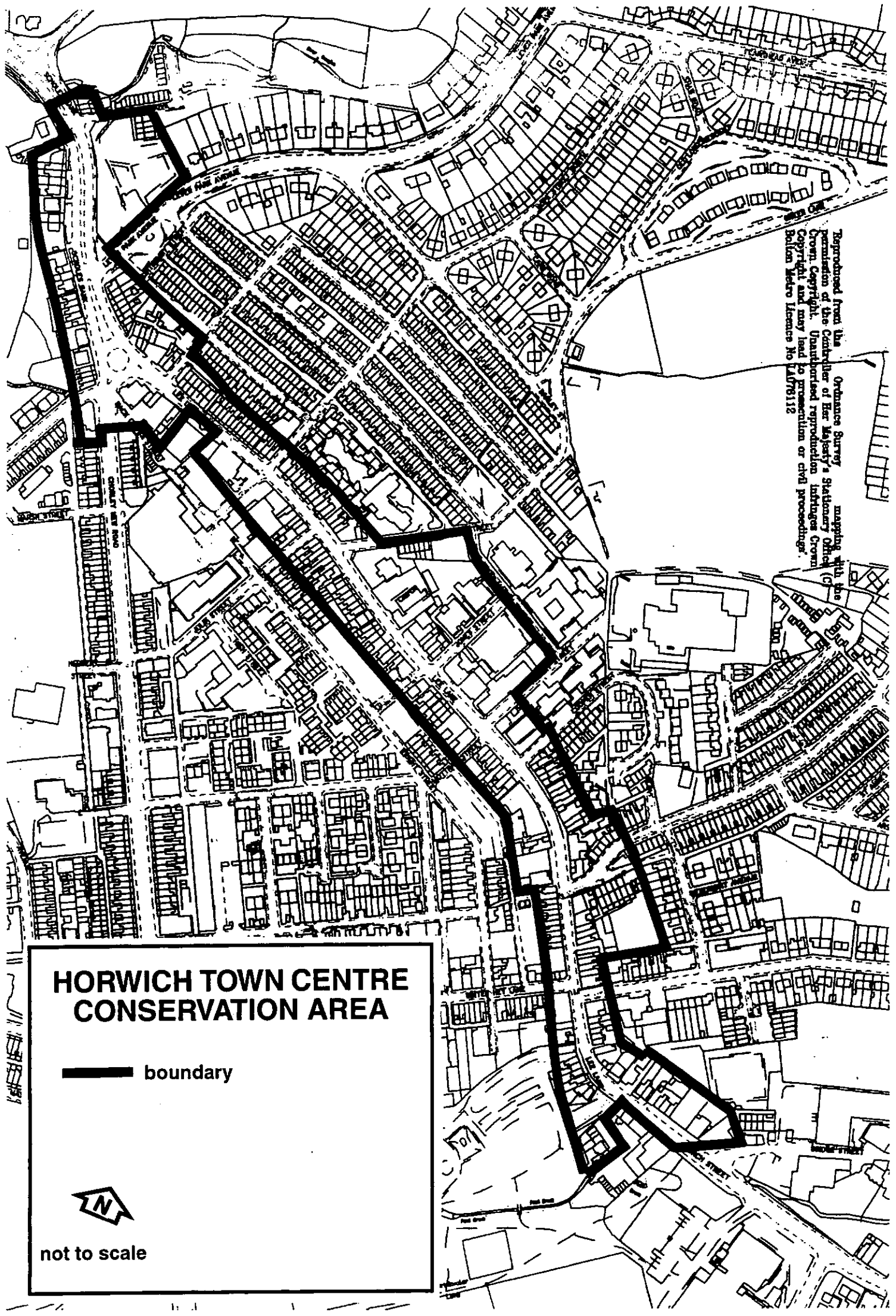
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HORWICH TOWN CENTRE CONSERVATION AREA

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not to scale



SECTION ONE

Character Assessment

Introduction

Horwich is a small industrial town which is set on the edge of the West Pennine Moors and is overlooked by Rivington Pike. The Town Centre Conservation Area is one of two in Horwich, the other being Wallsuches. The Town Centre Conservation Area includes the approaches to Horwich from the west and the local commercial centre on Lee Lane. It is linear in character. The main thoroughfare of Scholes Bank/Lee Lane forms the central spine of the Conservation Area. Many fine public buildings of local significance, and attractive stone cottages, are located within the Conservation Area but none are listed buildings. The area now identified as the Conservation Area was developed in the late Georgian and early Victorian period and most of the original buildings remain, albeit somewhat altered. As such the area is of special historical and architectural importance.

General background

The town of Horwich experienced two significant periods of development. One in the late eighteenth century and early nineteenth century associated with the growth of the

cotton and bleaching industries; Wallsuches Bleachworks which developed from 1780 is one such example. The second period of growth was at the end of the nineteenth century and beginning of the twentieth century, caused by the advent of the Horwich Locomotive works in 1884. The Conservation Area reflects the development of the town which is illustrated on the maps of Horwich at the end of this document.

The linear Conservation Area is too large and diverse to be described as a whole. It includes distinct localities shown on plan 1. These are considered individually in the following sections-

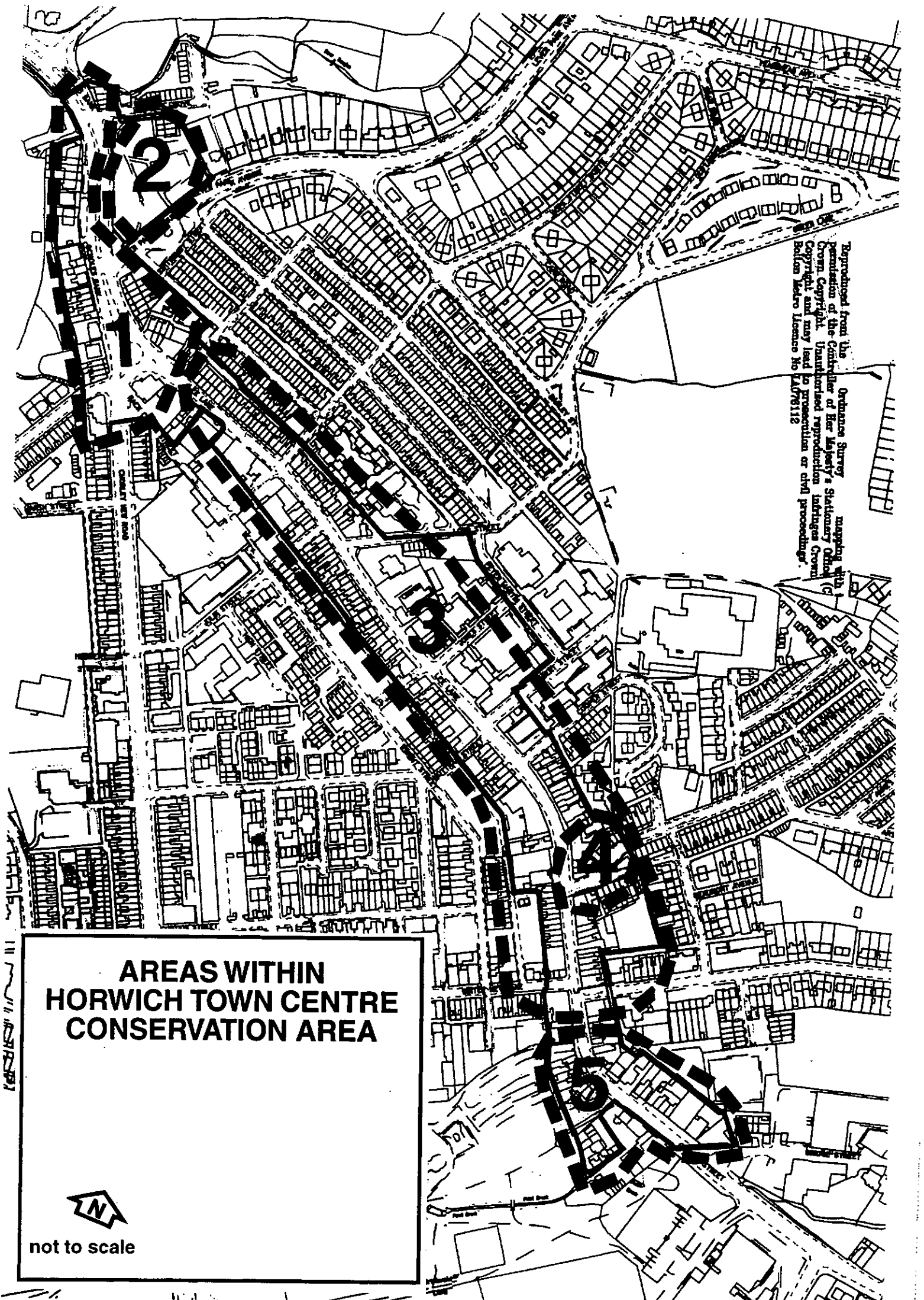
- 1 The approaches to the town from the west, Scholes Bank and Crown Square.
- 2 The Remembrance Park and Heavily Grove.
- 3 Lee Lane.
- 4 Sparrow Park.
- 5 The Station houses, Holdens Terrace and eastern environs.

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**AREAS WITHIN
HORWICH TOWN CENTRE
CONSERVATION AREA**



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1. The approaches to the town from the west -Scholes Bank and Crown Square

History

Scholes Bank takes its name from the steep bank up which it rises; "Scowl Bank" as it was originally known, because of the black scowling appearance of the promontory. The wide modern dual carriageway road of Scholes Bank was widened between 1946-1950. Crown Square is a triangle containing a large traffic roundabout at the brow of Scholes Bank. A toll house and gates were originally situated here. A magnificent Victorian horse trough and lamp post were located in the square but melted down for scrap during the last war.

Townscape, and detailed features

Scholes Bank is an imposing main entrance to the town from the west; it has been described in the book 'About Horwich' by M.D. Smith as "so impressive that it bespeaks a town of much greater proportions". It comprises a wide modern dual carriageway road with houses to the south side and The Remembrance Park on the north. The houses on the Bank are individual and varied. In particular Nos. 18 and 20, 12 and 14 have some attractive stone details. The characteristic and unifying feature of this area is the grand and open sweep of the road as it rises up to the town. (Fig 1)

Crown Square has visual importance being located at the major road junction at the beginning of the commercial centre of the town. It is overlooked by the Crown Hotel public house and flanked by two other pubs, the Toll Bar Inn and the Queens Head. At present the square has an open windswept aspect dominated by an extensive flat roundabout. The whole of this approach to the town is marred by an excessive number of intrusive traffic signs and lamp posts.



Figure 1.
Scholes Bank

2. The Remembrance Park and Heavily Grove

History

The Remembrance Park is a formal town park some 0.5 ha in size, laid out after the Second World War. Adjoining the park is the entrance to Lever Park, an extensive estate to the north of the town. On the Scholes Bank frontage are an impressive pair of stone obelisks which bear an inscription explaining that they were erected by the second Viscount Leverhulme, to commemorate his father.

Townscape

This area to the west of Scholes Bank warrants individual consideration as it differs in character from the rest of the Conservation Area. It consists of a formal town park and a small terrace of houses which are essential features at the approach to the town. (Fig 2)

THE LANDSCAPING OF THE PARK

The park is laid out with lawns, borders and ornamental trees. It contains a simple but effective rectangular yellow sandstone memorial and Garden of Remembrance for the men of Horwich who gave their lives during the Great War 1914-1918 and the Second World War 1939-1945. It provides a valuable green open area in contrast to the densely developed

terraced streets nearby and enhances the entrance to the town.

Architectural character

HEAVILY GROVE

Heavily Grove (Nos. 9,11,13, and 15) are a terrace of small cottages set in the park environment. They are typical of the domestic architecture of the early 19th Century. (Fig 3)

The front gardens are enclosed by a wall formed from stone flags which is a traditional local feature worthy of retention.

Details

Nos 9 and 13 are of dressed stone construction Nos.11 is watershot stone and no. 15 has rendered walls. All four houses have slate roofs, but only number 11 has retained its stone chimney and original pots. No original window or door fittings remain, but the openings are original except at 15 where a bay has been added. Nos 7 & 11 have horizontal openings at first floor and they may have been weavers cottages. Traditional style bollards have been erected on the street frontage.

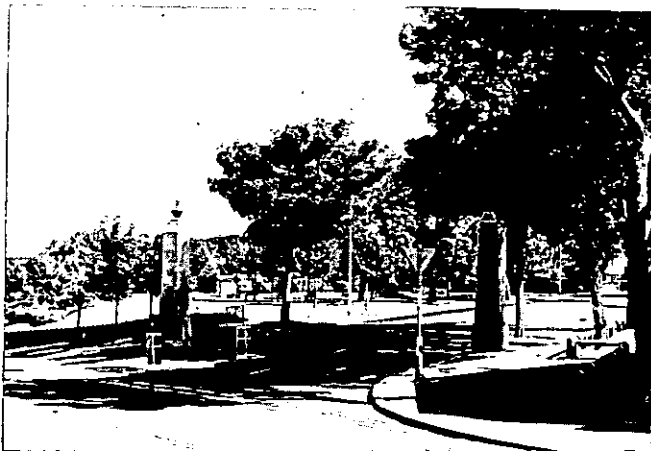


Figure 2.
Remembrance Park



Figure 3.
Heavily Grove

3. Lee Lane

History

Lee Lane has developed from an old pack horse road laid out between Adlington and Bolton in 1763 by the Nightingale Turnpike Trust. Hampsons History of Horwich 1883 states "Lee Lane was a narrow and undignified thoroughfare with some three or four houses at a considerable distance from each other." The development of Lee Lane took place throughout the 1800's, in an incremental manner, reflecting the development of the town as a whole. The pattern of growth is illustrated on the maps of Horwich at the end of this document. These show that initially development clustered round the Crown Square and Winter Hey Lane junctions. Gradually the Lane was built up so that by 1900 it was as we know it today.

Townscape

Lee Lane forms the heart of the Conservation Area. The street includes a wide range of architectural styles and building types, and this diversity contributes to the special character of the area. There are over 250

properties on Lee Lane alone, therefore it is not possible to describe every feature in detail. The main elements are drawn out below.

ENCLOSURES

Vistas are closed by virtue of slight bends in Lee Lane. This provides the sense of enclosure which creates an interesting and intimate character. It is most apparent at the junction of Lee Lane and Winter Hey Lane. Here the effect is reinforced by slight gradients, the steep bend at the beginning of Church Street, and the imposing three storey buildings at the junction. (Fig 4)

LANDMARKS, VIEWS AND SKYLINE

The area is situated below the West Pennine Moors which provide an imposing backdrop to the north. Side streets provide glimpses and attractive views to the north up to Rivington Pike. (Fig 5) Looking east down Lee Lane the tower of the Horwich Parish Church stands out above the roofs on the skyline.








Figure 4.
Lee Lane sense of enclosure



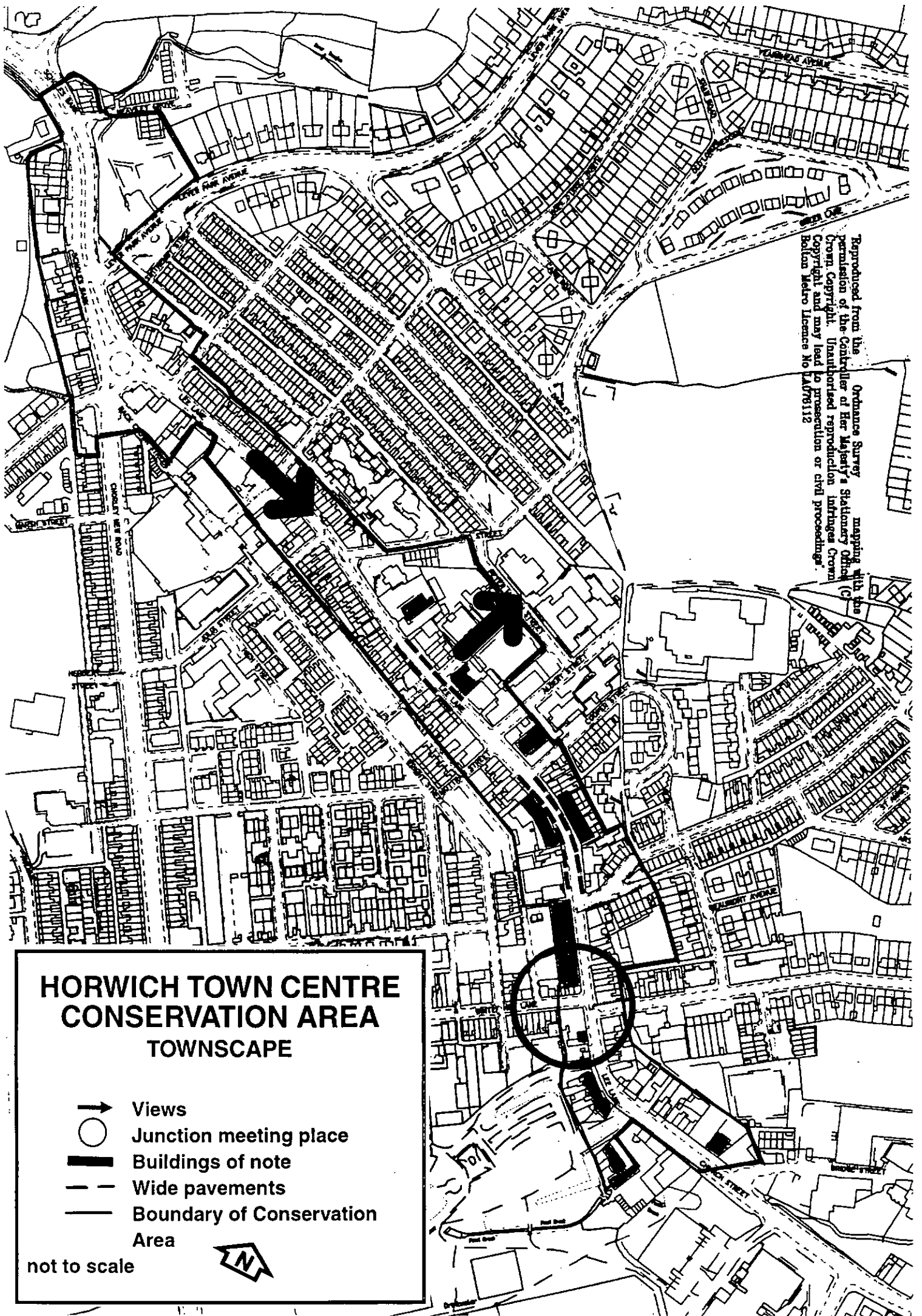
Figure 5.
View to Rivington Pike

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HORWICH TOWN CENTRE CONSERVATION AREA TOWNSCAPE

-  Views
-  Junction meeting place
-  Buildings of note
-  Wide pavements
-  Boundary of Conservation Area

not to scale



BOUNDARY TREATMENTS

Garden walls formed from stone flags are found in the row of houses 231-243 at the west end of Lee Lane. Walls built of dressed stone with rounded coping stones are also typical and are worthy of retention.(Fig 6 & 7)



Figure 6.
Stone flag walls Lee Lane.



Figure 7.
Dressed stone walls Lee Lane

STREET LAYOUT

Lee Lane takes a slightly meandering route which contributes to the character of the area. Side streets added later follow a more grid iron pattern, allowing clear views out.

JUNCTIONS AND MEETING POINTS

The junction of Lee Lane and Winter Hey Lane contains a concentration of commercial buildings and has a high level of pedestrian activity. The streets are quite narrow at this point as are the pavements. This constriction generates congestion and conflict between pedestrians and vehicles that needs to be resolved.

STREET FRONTAGES

The buildings do not adhere to one building line. Adjoining terraces of houses or shops vary in their distance from the carriageway which adds to diversity and interest. Of particular attraction are the dwellings with front gardens within the commercial centre. Pavement widths also vary. They are wide near the Public Hall and also further east on the bend in Lee Lane at Kwik Save and the old Co-op building. These areas where the pavements are wider form useful meeting points.

STREET FURNITURE

A seat has been provided at the junction of Lord Street and Lee Lane in front of the car park to Chapels Restaurant and provides a useful facility. It is not in a particularly attractive location or condition. Although some original stone flags remain most of the paving is in concrete and some areas need relaying. At the junction of Lee Lane and Winter Hey Lane new bollards and concrete and red brick paving have been provided. The bollards help to protect pedestrians but the new brick paving is not in keeping with the character of the area.

Architectural character

Lee Lane is made up of a variety of building types of differing architectural styles. The unifying and interesting feature is that the whole lane was built in the 1800's and remains virtually intact, albeit with superficial alterations. The main recent buildings are:

- Kwik Save,
- Lo Cost, Livseys Funeral Directors
- and the part new build part refurbishment to form Chapel's Restaurant.

The majority of the buildings are built in local stone from the Montcliffe Quarry, although later 19th century buildings are built of brick with stone details

The west of Lee Lane- domestic character

The west portion of the street is predominantly residential and comprises rows of stone built slate roofed terraced cottages. The houses are typical of South Lancashire and are not of an ornate style. Amongst the oldest are

nos.183-187 Lee Lane which were started in 1805.

DOOR ARCHES

Of particular interest are the different stone door openings. Three different styles feature, a four centered arch, semi circular arched head with central key stone, and stone lintels either plain or ornate. The four centered arch is uncommon. (Fig 8)

SHOP WINDOWS.

The shops in this area are small and also stone built; some still retain their original shop windows. There is no common style of window; each has a unique interest. (Fig 9)

Eastern part of Lee Lane - commercial area

Further east the properties are predominantly commercial, interspersed with public buildings. These are on a grander scale, many being three storeys high, constructed in a wide variety of materials. Travelling along Lee Lane from west to east, buildings and groups of particular note are described individually below.



Figure 8.
Four centred arch Lee lane



Figure 9.
Shop front Lee Lane

LEE HOUSE (NO. 146) AND NO. 152 LEE LANE
 Lee House, No. 146 Lee Lane was constructed in 1851 as the doctors house. The house was built as double fronted but has since been sub divided. It is constructed in a hand made red brick with stone quoins and slate roof and is unusual, being set in a terrace of stone houses. The window openings have stone cills and lintels and would originally have contained double hung sash windows. The doorway is wide with a semi circular brick arch. A stone carriage arch to the doctors stables is still visible behind the shop front that now exists at 146a.(Fig 10) No. 152 is also rather grander than the typical cottages, being a double fronted stone house, originally occupied by the dentist.

THE PUBLIC HALL

The Public Hall was built in 1878 with funds donated by the successful cotton magnate Peter Martin. He gave it to the town as a Temperance Hall. The bowling green at the rear still remains. The building is Gothic in style, constructed in red pressed brick with blue and yellow brick details, a string of decorated terra-cotta tiles, and an apex and details in Yorkshire stone. Perhaps the most striking feature of the building is the turret, surmounted by a weather vane from which project the points of the compass.(Fig 11) For a number of years the Public Hall was the focal point for all social events in Horwich, but when the Lancashire & Yorkshire Railway Co. opened the Mechanics Institute on Chorley New Road in 1888, for workers at the Locomotive works, its popularity began to decline.



Figure 10.
 Lee House



Figure 11.
 The Public Hall

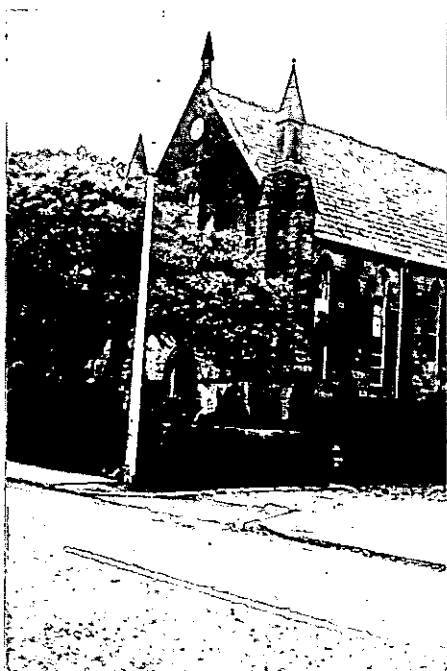


Figure 12.
Congregational Chapel

THE CONGREGATIONAL CHAPEL

The Congregational Chapel dates from 1856 and is the second to stand on this site. It was designed by George Woodhouse and built by Mr Pickersgill. It was also endowed by Peter Martin. A Sunday school existed next door, the remaining portion of which has been converted to Chapels Restaurant. The chapel is an imposing building constructed in local stone and slate, with a walled grave yard to the front and side. The trees and shrubs in the grave yard contribute to the street scene. (Fig 12)

THE NEW INDEPENDENT METHODIST CHURCH

The New Independent Methodist Church was erected in 1906. It was required because of the increase in population following the opening of the Locomotive Works. It is of an ornate Gothic style constructed in red pressed brick with yellow Yorkshire stone details. (Fig 13)

THE CO-OP STORES

The story of the Co-op in Horwich is an indicator of the changes in the town's fortunes. The first Co-operative shop was in the front room of a stone cottage, 126 Lee Lane and opened for business in 1862. Trade quickly

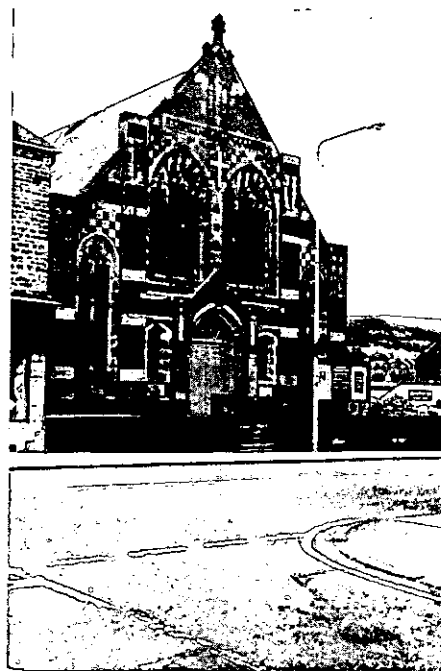


Figure 13.
New Independent Methodist Church

became established and a rental was taken out on a larger cottage shop at 140 Lee Lane. These new premises also proved inadequate and in 1865 a brand new central store was constructed at 116 - 120 Lee Lane. This attractive building still retains its stone name plaque at first floor but has been unsympathetically modernised at street level. The arrival of the Locomotive works brought unprecedented expansion and branches sprang up all over Horwich. In 1900 it was resolved to purchase a number of old buildings on Lee Lane and a new central store was



Figure 14.
Co-op New Central Store

erected in 1902. These extensive premises 86 - 96 Lee Lane included offices, various types of shops and a cloggers at the rear. The rather grand buildings are constructed in red pressed brick and Yorkshire stone, and have an elephant carved in the central stone gable. Of particular note is the recessed main entrance with canopy over. The Co-op has now gone, the premises are rather run down and occupied by a variety of users (Fig 14). Fig 15, Photo First Central Premises p90 About Horwich, M.D. Smith.



Figure 15.
Old photo of previous Central Store

UNION STREET

Nos 99 - 83 Lee Lane comprise a row of new shops erected in 1886 coinciding with the arrival of the Locomotive Works. They were built onto the front of a terrace of cottages on Union Street dated 1824. The original street name built into the side of No. 99 together with a cottage still visible behind No. 83 Lee Lane provide evidence of the original street layout. (Fig 16)



Figure 16.
Union Street and name plaque

MARKET BUILDINGS

The row of shops from 64 Lee Lane to the Winter Hey Lane junction were built on the site of Gaskells Mill in 1886, and are known as Market Buildings. These premises are three stories high with ornate Victorian brickwork. They have been unsympathetically altered particularly at street level. Numbers. 36 - 42 have been replaced with the modern 'Lo Cost' Supermarket. The buildings on the corner are very distinctive being constructed in yellow pressed brick with red courses. (Fig 17)



Figure 17.
Market building (showing coloured brickwork)

4. Sparrow Park

History

This area formed by the junction of Brownlow Road and Silverwell Street north of Lee Lane has both historical and visual interest. Brownlow Road originally formed the gateway to the Brownlow Estate, which included the Manor House and Rockhaven Castle. Rockhaven Castle was demolished in 1942 but contemporary photographs demonstrate that it was one of the most memorable features of the Horwich landscape, with a fascinating family story. Rockhaven Castle was developed from 1840 by Richard Brownlow, an attorney of Wood Street, Bolton. He was the son of Christopher Brownlow of Manor House, Horwich. Richard was a keen horseman who together with a Mr Munday of Preston founded Horwich Races. In later life Richard Brownlow became something of a recluse mainly due to a facial disfigurement; he did however continue to hold parties at the Castle but on these occasions he wore a face mask. Following his death in 1899 the Castle was purchased by Lord Leverhulme.

Silverwell Street formed the entrance to Top of the Meadow Farm. A smithy was located on the street and for that reason the narrow street was known as Nail Rock. Sparrow Park itself was formed by the demolition of a row of cottages at the bottom of Brownlow Road in 1890. The small area of trees and grassland this formed contained a field gun captured during the First World War, but this was taken to be melted down for armaments during the Second World War.

Townscape

The two streets enclosing this area each contain a single terrace of dwellings. The grouping of this terrace within the small park forms an interesting feature of townscape value

Sparrow Park is a triangular area which contains grass, shrubs, small trees, the base and post of an old cast iron gas lamp, and a post box, all enclosed by a low brick wall. It provides a tranquil green area directly adjoining the busy commercial centre of Horwich. The extremity of Silverwell Street still retains its original stone setts, but these contribute to the impression that this is an area that modern development has forgotten.



Figure 18.
Sparrow Park

Architectural character

The stone cottages on Silverwell Street are typical early 19th century domestic buildings. They are constructed in watershot stone. The majority of the original slate roofs have been replaced with concrete tiles. The window openings are small and the majority of window frames and doors have been replaced. These cottages also have front garden walls formed from stone flags. Brownlow Road consists predominantly of Victorian red brick slate roofed terraced houses, except No. 12 Brownlow Road a small stone house reputed to have been a Toll House.

5. Station houses and eastern environs

The east end of the Conservation Area lacks cohesion but contains several features of interest;

- the Station houses,
- the Station entrance,
- Holdens Terrace,
- Gorton Fold, the Conservative Club and the Bridge Inn.

This portion of the Conservation Area which includes Church Street, abuts the Wallsuches Conservation Area.

THE STATION AREA.

This area which contains the railway station entrance and adjoining houses Nos. 2 - 12, is worthy of individual consideration, because of the town's railway history and the architectural interest of the buildings.

History

In 1866 the Lancashire and Yorkshire Railway awarded a contract to Garside and Stead to construct a railway line connecting Hindley, Blackrod and Horwich including a railway station at Horwich. Prior to this the nearest station had been Blackrod railway station at Horwich Vale. Horwich Station opened to passenger traffic in February 1870. It is ironic that subsequently in May 1884 the Lancashire & Yorkshire Railway Co. purchased a site in Horwich to construct the Locomotive Works. This provided the main source of employment for the people of Horwich and district for almost a century. The stone entrance pillars to the station and the decorative cast iron arch above, remain today, although the station itself has now been demolished and a park created on the site. The name 'Horwich' was erased from the pillar and arch during the Second World War, and has never been replaced. The arch is also of interest as it contains an original Queen Victoria post box.

Nos. 2 and 4 Lee Lane comprise the station masters house and assistant station masters house, and are presumably contemporary with the station.

Townscape

The row of houses 2-12 Lee Lane are set back from and a little below the present level of Lee Lane and are enclosed by a low stone wall and original wrought iron railings. The pavement is of double width giving some shelter from traffic on the busy road.



Figure 19.
Station houses

Architectural details

The station houses Nos 2 & 4 are a pair of semi detached houses of charming and distinctive character. (Fig 19) They are constructed in dressed stone with slate roofs. The roof is a four sided pyramid with a central stone chimney stack that remains although the original pots have been removed. The houses are not square, the side walls splay out from the front elevation, and appear to have been constructed to fill a particular plot! The doorways are rather grand examples of the typical four centred arches. This house also contains an unusual small arched leaded light in the side elevation.

The doors and windows to both houses have been altered but they still retain their original wrought iron front gates.

The cottages Nos 4 - 12 (Holdens Terrace) are of typical domestic style. They are constructed in watershot stone with slate roofs. The original stone chimney stacks remain together with a number of attractive chimney pots and the original box wooden gutters on stone brackets. The cottages are unusual as they include not only wide stone lintels and cills but also stone jambs to the windows. This gives the row a very dour and solid appearance. They are some of the oldest buildings in the Conservation Area. The windows are unusually large, no. 6 retains the original style of fenestration and no 8 retains an old shop window opening. The row is attractive because very few details have been changed.

GORTON FOLD

The Fold is located at the eastern end of Lee Lane and only the cottages remain on the site; the Mill and barn having been demolished. The Mill built in 1714 lays claim to being the birthplace of the cotton trade. Pearl Brook drove the water wheel which powered machinery at the Mill. Several denominations including Roman Catholics, Unitarians and Methodists held religious services at Gorton Fold before their own places of worship were established. The cottages were rebuilt in the 1800s and are overshadowed by the Petrol Filling garage on the road frontage. This is a very attractive group of buildings particularly worthy of retention and any opportunities for improvement of the general environment should be utilised. (Fig 20)



Figure 20.
Gorton Fold

THE CONSERVATIVE CLUB, CHURCH STREET

The Conservative Club which is virtually at the eastern extremity of the Conservation Area, is a two storey building constructed of stone in 1887. It originally also carried a statue of Disraeli. The Poor Law office and the First Bank of Bolton were originally located in this building.(Fig 21)



Figure 21.
Conservative Club

THE BRIDGE INN

The Bridge Inn is an imposing building on a prominent corner site. It is three storeys high with a wealth of terra cotta detail.

SECTION TWO

Policy Guidelines

Statutory controls

Horwich Town Centre was designated as a Conservation Area in 1987. This means that:-

- (a) Conservation Area Consent is needed from the Council to demolish or part demolish all but the smallest buildings. Part demolition includes the removal of features such as chimney stacks and bay windows. It also includes the demolition of elevations of a building.
- (b) The Council must be notified in writing six weeks in advance of any proposal to cut down, top or lop a tree in a Conservation Area.

Bolton's Unitary Development Plan contains a number of policies relating to Conservation Areas. These are set out in the Appendix. Part of the Conservation Area lies within the identified shopping centre of Horwich and is subject to policy S5 relating to district shopping centres. A District Centre Policy Statement has been produced for Horwich. This provides general planning guidance for the centre but does not address specific conservation issues. Details are included in the Appendix.

General guidance on policy for Conservation Areas is given in Development Control Policy Note 19. Guidance on shop front design and advertisements is given in Development Control Policy Notes 4, and 6. These Notes are available from the Planning Department. Guidance leaflets on the care, maintenance and alteration of traditional buildings are also available free of charge.

Development guidelines

DEMOLITION

Consent will not be given to demolish buildings which make a positive contribution to the character of the Conservation Area. A significant element of the character of Horwich Town Centre is its completeness as a 19th Century development. In particular the residential terraces should be retained intact.

This policy will not apply to modern buildings which do not contribute to the character of the area. These include Kwik Save, Lo Cost and Liveys Funeral Directors, all on Lee Lane.

ADVERTISEMENT CONTROL

The Authority will use exacting standards when considering whether to grant consent for advertisements in the Conservation Area. Certain categories of advertisements are not permitted in Conservation Areas and discontinuance action will be taken against existing signs where they do not conform to the guidelines given in Development Control Policy Note 6. The Display of Signs and Advertisements.

NEW DEVELOPMENT

The sympathetic redevelopment of poor quality modern buildings will be encouraged. Any new development must reflect the character of existing buildings with respect to siting, scale proportions, material and detailing. Applications for Outline Planning Permission will not normally be considered.

STREET SURFACING AND FURNITURE

- Any original stone flags, setts and kerbs should be retained and relaid where this is necessary.

- New paving should be in reclaimed local stone or new stone. Concrete flags incorporating an appropriate aggregate would also be acceptable.
- Red brick paving should not be used as this material is not in keeping with the character of the area.
- Any alteration to the width of pavements should respect the townscape qualities highlighted in Section 1.
- Street furniture and signs should be kept to a minimum. Where they are necessary their style and location should reflect the character of Horwich.

Building Alterations MATERIALS

- Alterations should utilise traditional materials to match those used to construct the building. A variety of materials has been used in the Conservation Area though local stone from the Montcliffe Quarry predominates. Other traditional materials are pressed red brick, yellow brick and natural slate. Reclaimed local stone or new stone to match the existing should be used in preference to reconstituted stone.
- Strap or ribbon pointing should be avoided since this not only harms the appearance of the building but can damage the stone by preventing the run off of water.
- External walls should not be painted, rendered or clad in modern materials.
- External cleaning should only be carried out to remove corrosive dirt. Cleaning

should be carried out by a specialist firm under close supervision.

- Decorative features including plaques, mouldings and date stones should be retained together with original windows and doors.

WINDOWS AND DOORS

- Stone window cills, lintels and door surrounds, stone eaves brackets and stone steps should be retained together with any original windows and doors
- The integrity of the majority of terraced properties depends on an uninterrupted flat facade relieved only by the subtle modelling of the surface achieved by the recessing of doors and windows. The following are unacceptable:-

- porches;
- bow windows and bay windows (unless these are original features of the building);
- external shutters;
- changes in size or shape of window and door openings; and
- dormer windows on terraces which were originally designed without dormers. Any doorways or windows no longer used should be retained and not blocked up.

- Owners should be encouraged to use the following styles when replacing windows and doors.

Early Nineteenth Century Properties

- Vertical sliding sash windows with glazing bars in vertically proportioned window openings and side hinged casement windows in horizontally proportioned openings.
- either vertically boarded or six panelled doors.

Late Nineteenth Century Properties

- Vertical sliding timber sash windows.
- Four or six panelled doors.

Windows and doors should be made of timber and should be painted. Staining is not a traditional finish for timber and should not be used. UPVC windows and doors are not acceptable as they are not in keeping with the character of traditional buildings.

New windows or doors should be recessed to the original depth and should not be fitted flush with the face of the wall.

CHIMNEYS AND ROOFS

- Chimney stacks should be retained and if replacement is necessary this should be in the same materials which have been used to construct the remainder of the building. This may be brick or stone with clay pots. Where central heating flues are installed these should be contained within the original chimney pot or a traditional replacement.
- Roof repairs or replacements should be in slate. Where ridge tiles need replacing these should be in stone or blue clay.
- New rooflights may be acceptable but these should be flush fitting and should not be on prominent roof slopes.

RAINWATER GOODS

Replacement rainwater goods should be in cast iron or moulded aluminium with a black coating.

GARDEN WALLS AND GATES

Stone garden walls including upright flags should be retained. Flag walls are a traditional local feature on Silverwell Street, Heavily Grove and the west end of Lee Lane (Nos 231-243). Any repairs needed should be carried out using the same materials and methods of

workmanship. Original wrought and cast iron garden gates are unique and should be retained. Examples are the Station Houses, 2 and 4 Lee lane and 141 Church Street.

COMMERCIAL PROPERTIES - SHOP FRONTS AND ADVERTISEMENTS

- Decorative plaques, mouldings, signs and date stones and any other original features of interest should be retained.
- Traditional Victorian shop fronts and shop windows should be retained and repaired where practicable.
- The installation of traditional style painted shop fronts will be encouraged. This will include the use of stall risers, pilasters and cornices to frame the shop windows. Hand painted fascia signs and hanging signs are also in keeping with the traditional character of the street and will be encouraged.
- Advertisement consent will not be given for internally lit projecting signs. The use of external lights and concealed lighting will be encouraged.
- Security shutters should preferably be fixed inside the shop window and be of a perforated grille type to allow for a view of the window display. If external shutters are unavoidable, they should be incorporated into the overall design of the shop front with the shutter box concealed behind the fascia. The shutter system should be colour coated to match the colour of the shop front and the shutters should be perforated.
- Externally fixed shutter boxes which project from the facade of the shop, galvanised finishes and solid shutters are not in keeping with the character of

the Conservation Area and are not acceptable.

MINOR FEATURES

Standard external fixtures including satellite dishes, meter boxes, burglar alarms, central heating flues and security cameras should be sited in unobtrusive positions. Wherever

possible they should be colour coated to match the background material i.e. walls or roof.

WHEELIE BINS

The layout of traditional properties can create difficulties for storing bins. Wherever possible, they should be stored out of sight not left out on the street or footway.

**CROWN SQUARE
LANDSCAPING**

**SEAT AT LORD STREET
REQUIRES REPLACEMENT**

**IMPROVEMENTS NEEDED
TO SPARROW PARK**

**TRAFFIC MANAGEMENT
REQUIRED**

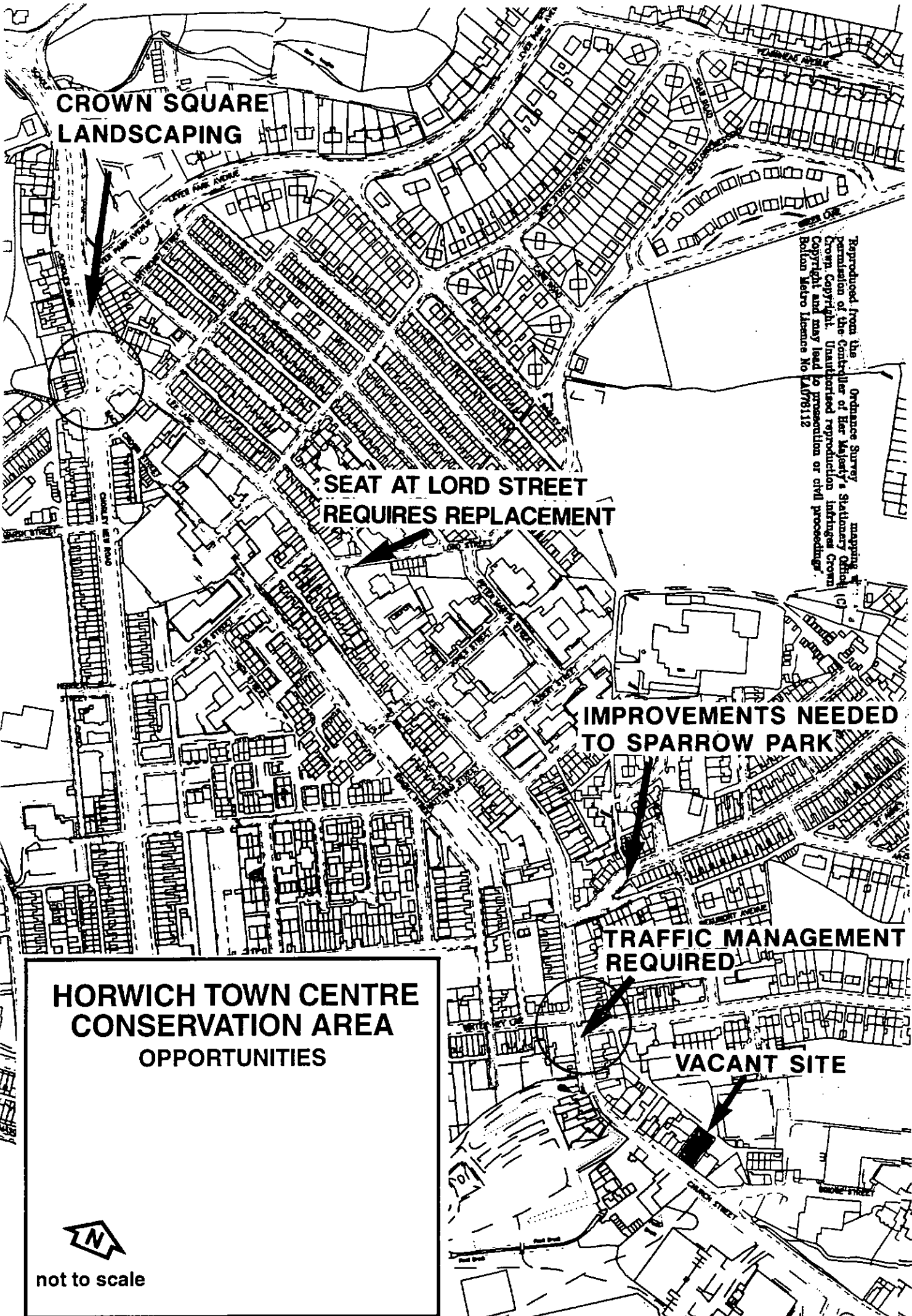
VACANT SITE

**HORWICH TOWN CENTRE
CONSERVATION AREA
OPPORTUNITIES**

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SECTION 3

Opportunities for Enhancement

This section highlights issues. It does not put forward detailed proposals for enhancement.

Scholes Bank and Crown Square

The area should be protected and enhanced by appropriate landscaping and the control of traffic sign clutter. At Crown Square hard and soft landscaping, including unifying feature(s), should be provided. Photographs of the area in circa 1900 show a large Victorian lamp. It would be desirable to replace this. (Fig 22)



Figure 22.
Crown Square where landscaping is required

Remembrance Park and Heavily Grove

The formal character of the Park should be protected and enhanced. The original features that remain such as the wall, stone pavement, flags and kerbs should be protected.

Sparrow Park

There is scope for replanting the garden generally, providing improved hard landscaping and repairing the old gas lamp.

Station area

Considerable local improvement would be achieved by repairing the railings and relaying the paving in this area. Consideration should be given to reinstating the Horwich name on the station entrance.

Gorton Fold

Opportunities should be taken to enhance Gorton Fold. The relocation of the petrol station would make a major improvement. The preparation of a design brief for Gorton Fold would provide appropriate detailed design advice and set priorities for resolving conflicts between adjoining land uses. A face lift scheme would bring about considerable improvement to the commercial buildings on Lee lane and any opportunities for funding should be utilised.

Street surfacing and furniture Lee Lane

The area at the junction of Lord Street and Lee Lane would benefit from improved landscaping and a new seat. Uneven pavements should be relaid and any original stone kerbs and flags retained. At the junction of Lee Lane and Winter Hey Lane new bollards and concrete and brick paving have been provided. The bollards help to protect pedestrians but the new paving is inappropriate in style and material. Hard landscaping, which is in keeping with the character of the area, would provide considerable improvement. There is potential for a Horwich style and code of practice on street furniture to co-ordinate the street scene.

Traffic management

Lee Lane has experienced a considerable increase in heavy through traffic which is intrusive, dominating and detrimental to the character of the area. A traffic management scheme is presently under consideration and this together with appropriate hard landscaping would make a radical improvement to the area. Traffic management measures should be sensitive to the character of the area, and should be designed in consultation with the Planning Division.

Vacant sites

A vacant site exists next to 133 Church Street at the east end of the Conservation Area adjoining the Conservative Club where the Unitarian Church has been demolished. This section of Church Street has little cohesion. The vacant site provides scope for appropriate new development and a design brief for the locality including Gorton Fold would provide useful guidance.

Car parks

Car parks exist at the rear of the library on Lord Street and off Beaumont Road within the Conservation Area. Both require improved surfacing, entrances and signing. The car park at the rear of the Crown Hotel would benefit from resurfacing.

Extra controls over building alterations

Many properties both domestic and commercial have been considerably altered to their detriment. The rows of terraced houses are under immediate threat of alteration as evidenced by the scale and pace of inappropriate alterations that have taken place in the area to date. They would therefore benefit from the extra planning control provided by an Article 4 Direction. This would remove permitted development rights on



Figure 24
Shop front in need of repair

certain works such as changes to doors windows and roofs. It could also encourage owners to restore original features.

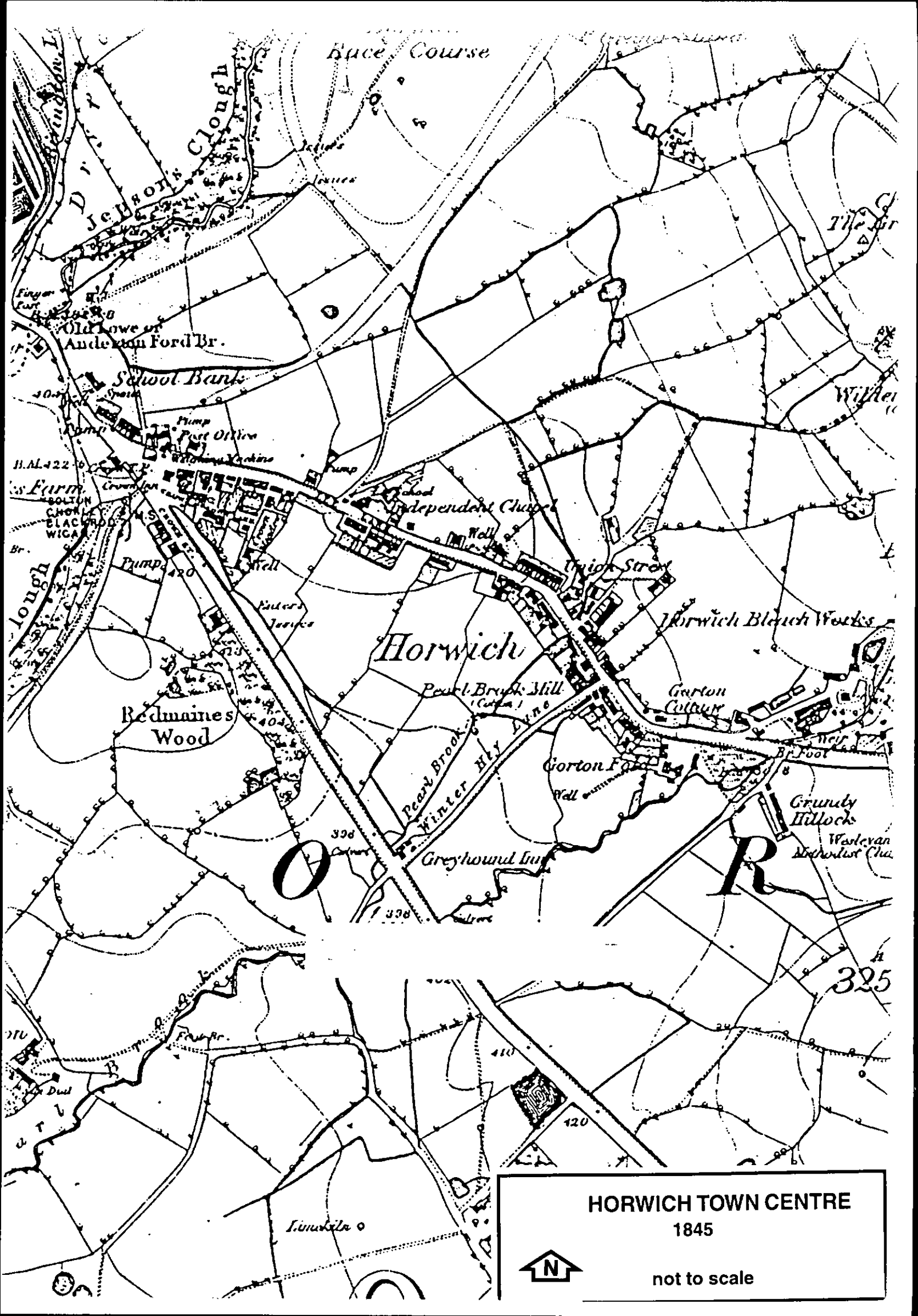
The Public Hall and Gorton Fold may be worthy of inclusion in the list of buildings of special architectural or historic interest in order to afford them a greater measure of protection. This matter should be investigated.

Shop front and advert design guides.

Horwich as a traditional small Victorian town could be greatly enhanced by the promotion of traditional shop front design. A leaflet giving advice to applicants should be prepared. This should identify existing shop fronts and shop windows worthy of retention.

Enforcement.

The status of existing signs and adverts should be reviewed and action taken where necessary.



Race Course

J. J. BROWN'S Clough

Old Lane of Anderson Ford Br.

School Bank

Pump Post Office

H.A. 422 0

Farm

Br.

Clough

Redmaines Wood

Horwich

Pearl Brook Mill

Peew Brook

Winter Bly Lane

Greyhound Inn

Union Street

Horwich Bleach Works

Gorton Colliery

Gorton Ford

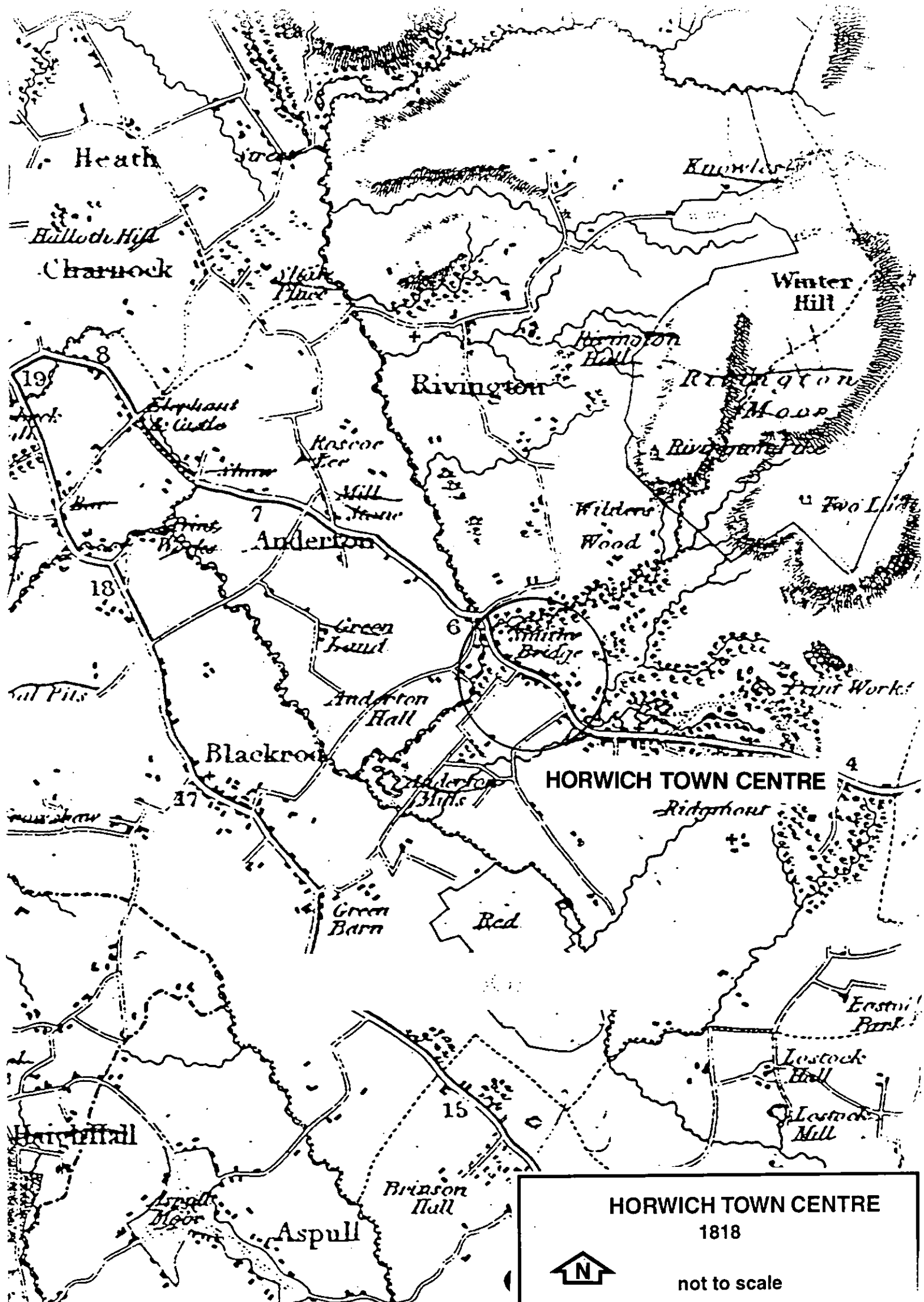
Grundy Hillock

Wesleyan Methodist Ch.

HORWICH TOWN CENTRE
1845

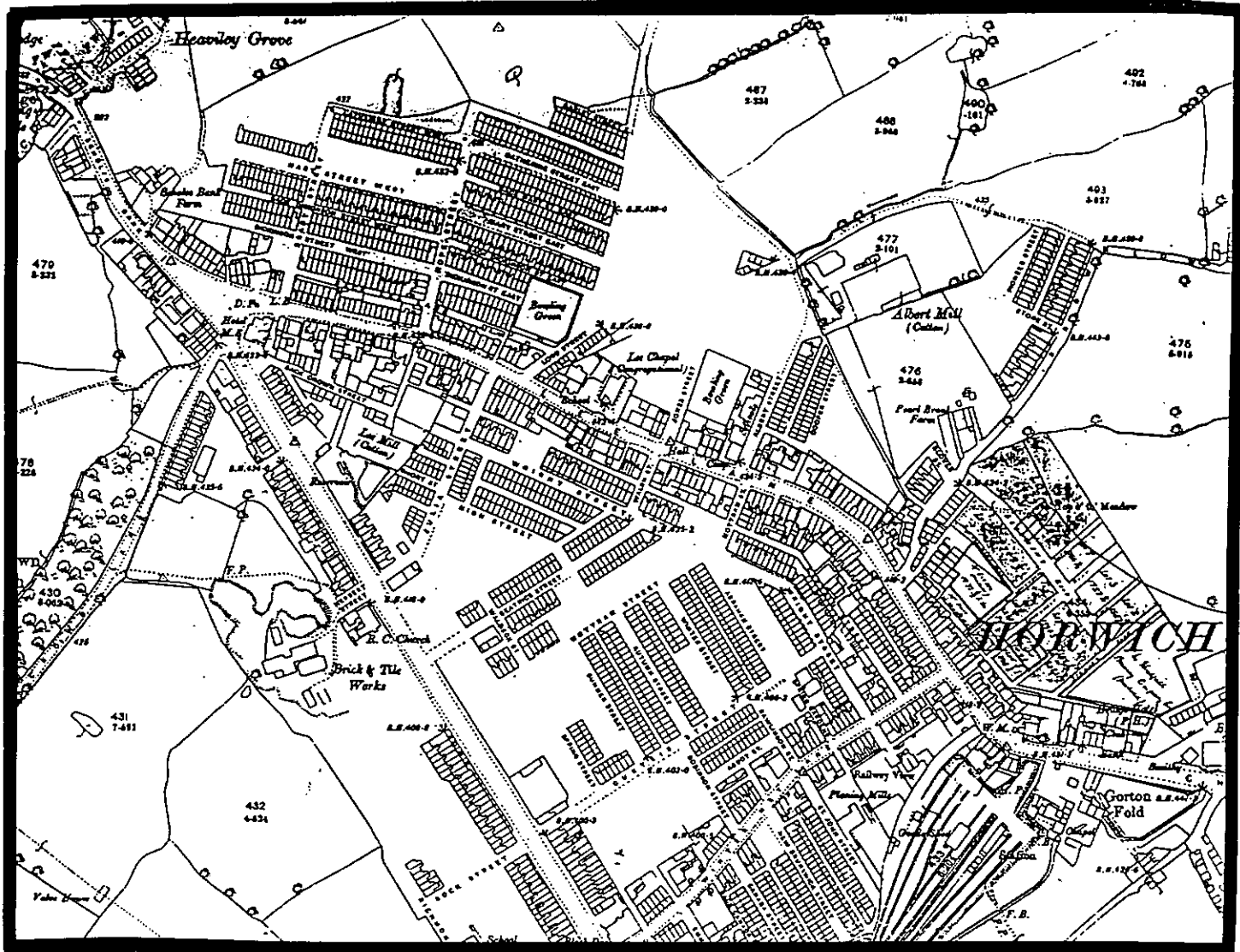


not to scale



HORWICH TOWN CENTRE
1818


not to scale



HORWICH TOWN CENTRE

1892



not to scale

APPENDIX

Bolton's Unitary Development Plan Policies

Conservation Areas

CE2. The Council will preserve or enhance the character of conservation areas.

Conservation areas represent a significant element of Bolton's architectural and historical heritage. The Council will preserve or enhance these areas through the control of development and through positive schemes of enhancement.

Development which is allowed should contribute positively to the quality of the environment. The Council is empowered to designate further conservation areas and will consider designating them as appropriate.

CE2/1 The Council will preserve or enhance the character of conservation areas by:

- (a) ensuring that all new development and alterations to existing buildings preserve or enhance the appearance of the conservation area;**
- (b) requiring the height, size, design, materials, roofscape and plot-width of new development, including alterations or extensions to existing buildings, to respect the character of the conservation area;**
- (c) seeking to retain the materials, features, trees and open space which contribute to the character of the conservation area.**

This policy outlines some of the elements which the Council will take into account when considering applications in conservation areas.

CE2/2 The Council will not normally allow the demolition of buildings which

contribute to the character of a conservation area. Where demolition is acceptable the Council will ensure that new development preserves or enhances the appearance of the conservation area, and that building takes place as quickly as possible after demolition

Consent from the Council is necessary for the demolition of buildings in conservation areas. If the building is important to the character, then demolition will be resisted. The Council will ensure that unsightly gaps are not left when demolition does take place by ensuring rapid redevelopment with good quality buildings.

CE2/3 The Council will not normally consider outline planning applications for development in conservation areas.

The Council will consider whether it has sufficient information to assess fully the environmental implications of a proposed development from an outline application.

District Centres

S5 The Council will normally permit proposals for shopping development, which are appropriate in scale and character, in or immediately adjoining the district centre

There are four district centres in the Borough namely Astley Bridge, Farnworth, Horwich and Westhoughton. They are smaller centres than Bolton, and offer local people convenient shopping for a wide range of goods. The Council wishes to sustain and support the role of the district centres as a principal focus for retailing, arts and cultural provision, leisure and entertainment, commercial services and community facilities. Support will be given to

proposals involving the refurbishment of existing centres and the council will, where possible, promote schemes which improve the overall shopping environment.

In addition the Council believes that there is opportunity to extend facilities within district centres, especially for non-food retailing. It will therefore encourage proposals which will enhance the range of goods and services available within the centre. Proposals will normally have to be in or adjoining the identified shopping centre. The Council will interpret "adjoining" to mean immediately next to the shopping centre boundary. However should a proposed site be separated from the shopping centre by a highway the Council will use its discretion as to whether the site is

adjoining. A major factor in the analysis will be the class of the road, and whether it is perceived as a boundary which signifies a change in land-use. The Council will consider preparing more detailed strategies or Action Plans for each of the four centres, and over the Plan period, the Council envisages the following developments:

HORWICH

The Council hopes to redirect through traffic, and particularly lorry traffic, away from the shopping centre to improve pedestrian safety and enhance the environmental qualities of the area. This may provide the opportunity to introduce a pedestrian priority scheme. The Council will also consider measures to improve car parking facilities.